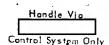
SECRET Approved For Release 2002/06/13 : CIA-RDP68B00724R000100+30005-6

25X1A		Copy 8 of 8 21 August 1969
25X1A 25X1A	MEMORANDUM FOR: ATTENTION: SUBJECT: REFERENCE:	Comptroller, OSA Impact of FY-70 NRO Funding Reductions Memorandum from DSA 8966-69) dated 19 August 1969. Subject: Same.
25X1A 25X1A	"Items f a. The would result in mission generat with existing dir no apparent inte reduce the IDEA further reductio	g Deputy for Operations comments are forwarded ced memorandum: or consideration and further study:" proposed four day work week at a reduction of the operational cion capability. This would not be consistent rections in the NRO funding reductions, i.e., and in the NRO budget reduction proposal to ALIST/TACKLE operational capability. In an considerations, the first consideration should con-operational related activities.
	c. Many manpower surve The team conclugained, follow-contact manning documents the FY-70 NRO the Detachments	cower review: In December 1968, an OSA by team conducted a review of Detachment G. ided: "As experience in U-2R operations is on manpower surveys will be required to insure cuments reflect optimum manning for the new "The recent study exercise to comply with budget approvals would seem to have placed in an optimum manning situation at this time. Evey would seem more timely subsequent to

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IDEALIST/TACKLE

any forthcoming mission posture changes for the Detachments, i.e., how can we best reduce our manpower to accomplish a

new or restatement of mission.

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25X1A	-	Page 2			
25X1A -	and It is conceivable that satisfies of a definitive study of the support	ort aircraft requirements ective Detachment Commanders.			
25X1A	the Detachment Commanders would suggest during a "belt tightening" review conducted on the subject. e TDY concept: A TDY concept would				
25X1A	have a significant bearing on De be considered as a last resort n addition, close coordination of a personnel atshou Agency and USAF personnel plat the Air Force would not favor th	nethod of operation. In a change in PCS vs. TDY ald be conducted between the nners. It is conceivable that			
25X1A	to their earlier concerns for remote assignment considerations for USAF personnel and the resultant effect on SEA assignments both prior to and subsequent to duty assignment.				
	2. This office will continue to so NRO funding requirements. You will be on the subject.	study means of reducing OSA e kept advised of any new thoughts			
25X1A					
		Deputy for Operations, OSA			
25X1A	Attachment: Paper on U-2/G storage				
	SAS/O/OSA (21 August 1969) Distribution: 1 - COMPT w/att.				
	 2 - D/OPS w/att. 3 - D/M w/att. 4 - SS w/att. 5 - D/SA w/att. 6 - CMD w/att. 				
25X1A	7 - BFD w/att. 8 - RB/OSA w/att. IDEALIST/TA	GROUP 1: EXCLUDED FROM			

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Α	Τ.	FACHMENT	TO
		9086-69	
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25X1A

15 August 1969

MEMORANDUM FOR: Colonel Price

SUBJECT

25X1A

Retirement of U-2C/G Models

REFERENCE

1721 (RYBAT) Same Subject

25X1A

1. Agree that long term money savings can be realized by recommendation but:

Reference paragraph 5 of cited message:

FYI: From 1 August 1955 to 5 January 1969, 49 U-2 aircraft have been involved in accidents. Of this number 7 were repaired and returned to service. Also of the total 7 aircraft are coded as lost to hostilities.

Statistics:

(1) Aircraft accidents by altitudes:

0 - 10001	16 (5 repaired)
1,000' - 10,000'	1
10,000' - 20,000'	1
20,000' - 30,000'	3 (1 repaired)
30,000' - 40,000'	4 (1 repaired)
40,000' - 50,000'	1
50,000' - 60,000'	1
60,000' - up	$\frac{15 \text{ (excluding 7 to } }{\varphi_2} \text{ hostile action)}$

(2) Training losses:

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23 accidents during training operations of which:

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	4 occurred on pilot's first U-2 flight.
•	l occurred on pilot's first high altitude flight.
	13 of the pilots involved had less than 50 hours in U-2 aircraft.
•	2. With this history, use of R assets for training not considered adviseable. Also restricting training in "C" model to 35,000 and below not supported by figures in 1B(1) above.
25X1A	3. Reference paragraph 6 of1721. This rationale would appear to increase expenditure of overtime, which at this time is not the route we are proposing.
•	
·	25X1A
	AD/OPS